



# **Southwest Recreation Center Weight Room Expansion**

**UF PROJECT NO: UF-670**

**UF BUILDING NO: 0316  
UNIVERSITY OF FLORIDA  
3150 HULL RD  
GAINESVILLE, FLORIDA 32611**

UF-670 Southwest Recreation Center  
Weight Room Expansion

CORE (Center for  
Outdoor Recreation...)

Proposed 7000 SF  
Building Expansion

TREE IMPACT

Southwest  
Recreation Center

Tennis Courts

UF Park and Ride Lot 1

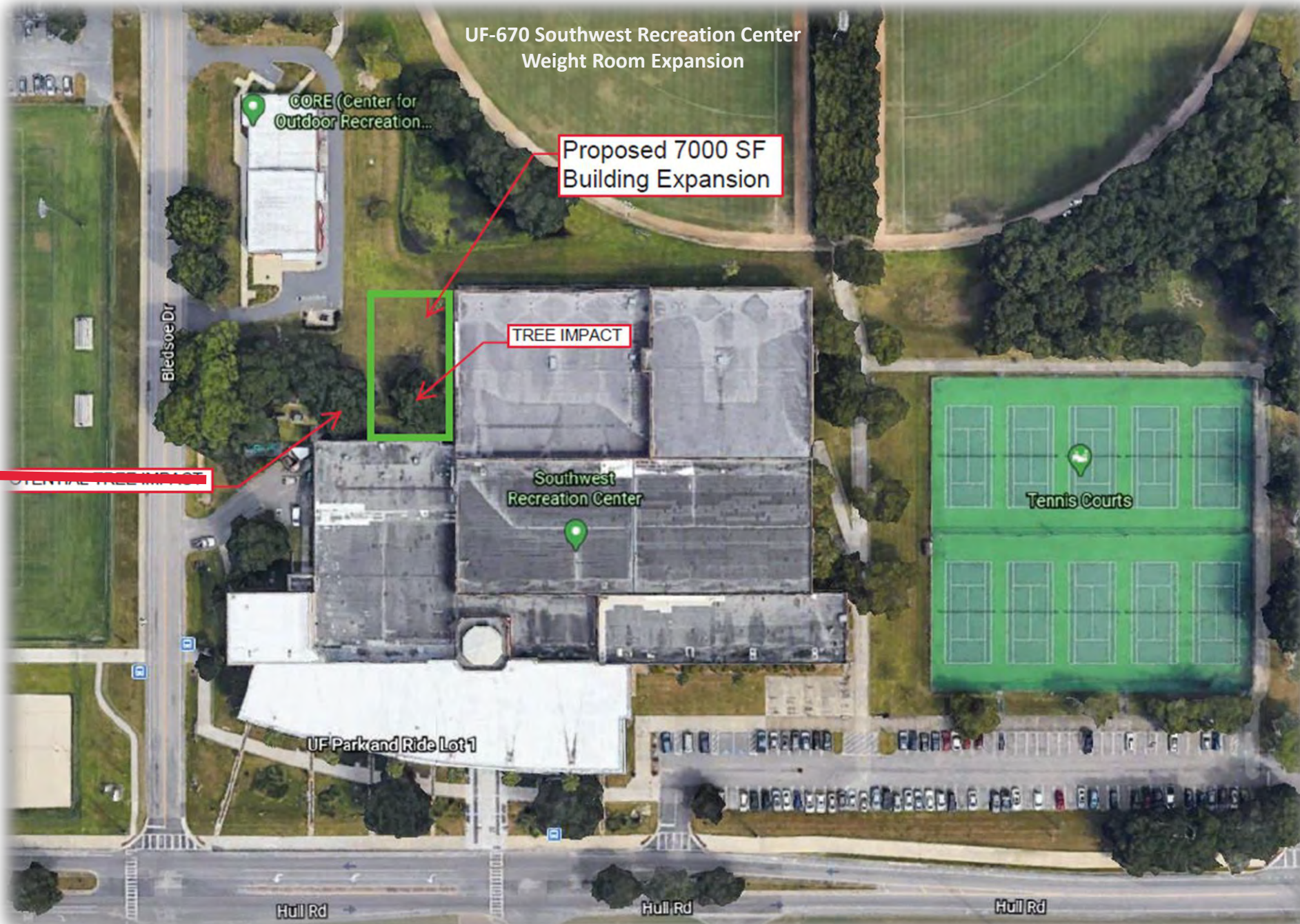
Hull Rd

Hull Rd

Hull Rd

Bledsoe Dr

POTENTIAL TREE IMPACT







Southwest Recreation Center  
Weight Room Expansion



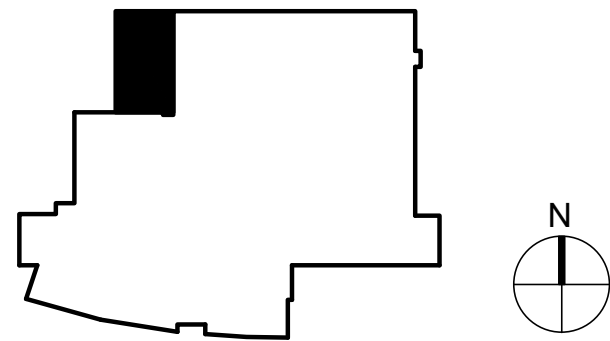
SUBMITTAL:		ISSUE DATE:
SCHEMATIC DESIGN		06/30/2021
REVISION	DESCRIPTION	DATE

**UF-670 SW Rec  
Addition**



3150 Hull Rd  
Gainesville, FL 32611

KEY PLAN

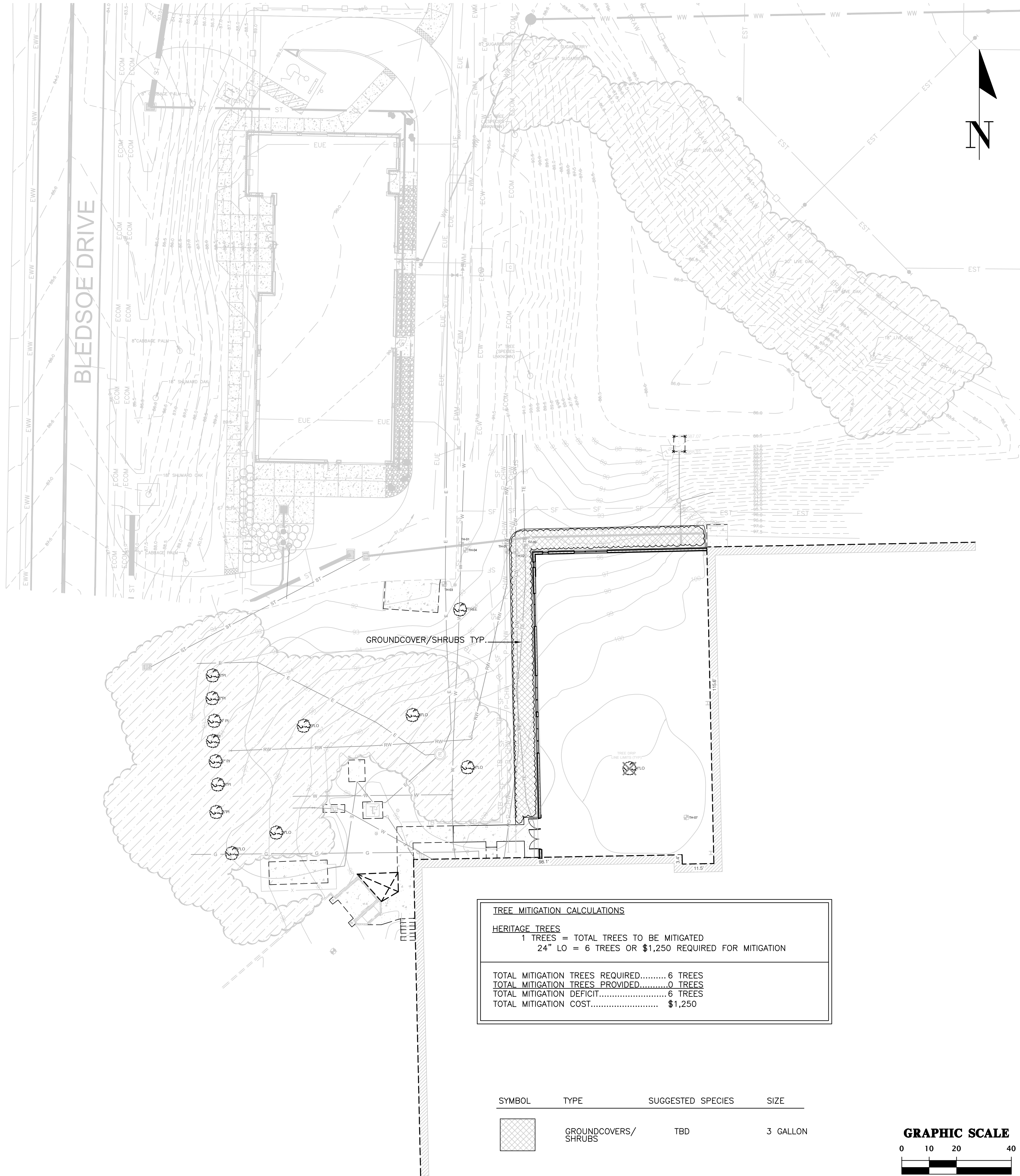


DRAWING TITLE:

**CONCEPTUAL LANDSCAPE  
PLAN**

PROJECT NO.:	21-0153	DRAWN BY:	CT
		CHECKED BY:	LH

**L - 101**



QUESTIONS ?

## REPORT TO THE LAKES VEGETATION AND LANDSCAPING COMMITTEE

<b>TO:</b>	The LVL Committee	<b>FOR:</b>	August 12, 2021 LVLC meeting.
<b>VIA:</b>	Carlos Dougnac, Assistant Vice President, PDC	<b>FROM:</b>	Cydney McGlothlin, University Architect
<b>REQUESTOR:</b>	DCP	<b>PRESENTERS:</b>	Cydney McGlothlin and User Group

PHASE:	Committee Responsibilities:	STATUS AND PRIOR COMMENTS:	DATE:
X PROGRAMMING	<i>The committee will review and recommend approval/denial of general site suitability - having evaluated impacts to trees, landscape, natural areas, and lakes.</i>		
SCHEMATIC DESIGN	<i>The committee will review and recommend approval/denial of tree removal - plans for transplants, replacements and/or mitigation, based on the building footprint, utility corridors, and other construction activities.</i>		
DESIGN DEVELOPMENT	<i>The committee will review and recommend approval/denial of final landscaping - appropriateness and inclusion of any mitigation for tree removal.</i>		

NOTE TO PM: All landscape plans and tree protection drawings shall illustrate the full (mature) canopy of trees, not just a dot or small circle.

**BACKGROUND INFORMATION:**

**PROJECT:**  
UF-653, Architecture Building Renovation/Remodeling and DCP Collaboratory

**SITE:**  
Existing Architecture Building and the area adjacent. See attached location map.

**STATUS:**  
The project is in the programming phase

**OBJECTIVES:**

- Approval of site
- Approval for 7-9 trees to be removed

**PROJECT PHASE AND PRESENTATION NARRATIVE:**  
Programming

**ENCLOSURES:**

1. Presentation
2. CMP Checklist

**Campus Master Plan Checklist**

To: ULUFPC, LVLC, PHBSC, P&TC DATE:                      PROJECT: UF-653 Architecture Building & DCP Addition  
Prepared by: Rachel Mandell FROM: UF Project Manager

This form is to be completed for the applicable phase at the time that the project is reviewed by committees. Do not mark shaded cells in the columns because they do not apply to the review at the specified phase. Checklists should be cumulative so that projects presented at Design Development have all phase columns completed. Design-build projects may omit the Schematic Design phase column. These checklist criteria apply to development on the main campus and, as applicable, on Satellite Properties in Alachua County.

EVALUATION CRITERIA	PROGRAMMING AND SITE SELECTION			COMBINE FOR DESIGN-BUILD					
				SCHEMATIC DESIGN			DESIGN DEVELOPMENT		
				<input type="checkbox"/> Concept	<input type="checkbox"/> Advanced				
	YES	NO	NA	YES	NO	NA	YES	NO	NA
<b>UNIVERSITY LAND USE AND FACILITIES PLANNING COMMITTEE (ULUFPC)</b>									
1) The project appears in the Capital Improvements Element, Table 13-1 (Ten-Year Capital Projects List) and Figure 13-1 (Future Building Sites) <input checked="" type="checkbox"/> As presented in the adopted Campus Master Plan <input type="checkbox"/> With edits to Table 13-1 to modify the project GSF or description <input type="checkbox"/> With edits to Figure 13-1 to modify or assign the project site	X						-	-	-
a) If "no" or with edits: The addition or modification of the project in the CMP can be accomplished as a Minor Amendment (per UF Operating Memorandum) and without changing the Campus Development Agreement							-	-	-
2) The project is consistent with the Future Land Use designation and definition ( <i>Figure 2-1, Future Land Use and Policies 1.1.2 and 1.1.8</i> )	X						-	-	-
a) If "no", the necessary modification to Figure 2-1 (Future Land Use) can be accomplished as a Minor Amendment (per UF Operating Memorandum) and without changing the Campus Development Agreement							-	-	-
3) The project location is consistent with policies that direct the location of specific uses (i.e. academic facilities, support/clinical facilities, housing, recreation/open space & parking) ( <i>Academic Facilities, Policy 1.2.3; Support/Clinical, Policies 1.1.3, 1.1.4 and 1.1.6; Housing, Policy 1.3.1; Recreation/Open Space, Policies 1.3.1 and 1.3.3; Transportation Policy 2.5.4 and 2.5.6</i> )	X						-	-	-
4) <input checked="" type="checkbox"/> The project is not a temporary building; OR <input type="checkbox"/> The temporary building is located in the Surge Area, Energy Park, Physical Plant Division complex, Academic/Research-Outdoor Future Land Use, or the temporary building supports construction activity ( <i>Capital Improvements, Policy 1.1.15</i> )	X			-	-	-	-	-	-
5) The project considers life-cycle costing, pursues principles of sustainable design and/or seeks LEED certification ( <i>Capital Improvements, Policy 1.1.14</i> )									
6) The building footprint, orientation and setback comply with Policy 1.3.1, Urban Design Element because the project is located with road frontage along Stadium Rd (Gale Lemerand Dr to Buckman Dr), University Ave (Gale Lemerand Dr to SW 13 <sup>th</sup> St), SW 13 <sup>th</sup> St, Center Drive, Museum Rd (west of Center Dr. to SW 13 <sup>th</sup> St), Archer Rd/SW 16 <sup>th</sup> Ave, or Radio Rd; or within new centers of development (i.e. near Orthopaedics & Sports Med, Cultural Plaza, Southwest Recreation, and near Fifield Hall)									



**Campus Master Plan Checklist**

EVALUATION CRITERIA	PROGRAMMING AND SITE SELECTION			COMBINE FOR DESIGN-BUILD					
				SCHEMATIC DESIGN			DESIGN DEVELOPMENT		
	YES	NO	NA	YES	NO	NA	YES	NO	NA
7) The project is a minimum of 3-stories; <u>OR</u> the project demonstrates unique programmatic, functional or code requirements that dictate a variance from the 3-story minimum; <u>OR</u> the project meets alternate building height and design characteristic requirements based on its location in unique areas of campus for which more specific building design requirements apply (i.e. near Orthopaedic & Sports Med, SW Research Circle/Cancer-Genetics area, Fifield Hall area, Cultural Plaza, Radio Road Commuter Lot area, Archer Road Corridor/Planning Sector "G", Historic Impact Area, PKY Developmental Research School and Eastside Campus) ( <i>Urban Design, Policy 1.3.4 through 1.3.10</i> ); <u>OR</u> the project meets guidance for building height and design of housing facilities ( <i>Housing, Policy 1.3.2</i> )									
8) The project provides community design integration along campus perimeters as described in Policies 1.2.1 and 1.4.3, Urban Design Element, with respect to landscaping, hardscaping, views, signage, and bicycle/pedestrian accommodation as applicable because the project is located along Gateway Roads identified in Figure 1-6, Urban Design Element (i.e. University Ave, SW 2 <sup>nd</sup> Ave, SW 13 <sup>th</sup> St, Archer Rd, and SW 34 <sup>th</sup> St)	-	-	-						
9) <input type="checkbox"/> The project includes exterior public art; - <i>Note: LVLC and PHBSC (if applicable) approval recommendation required</i> <u>OR</u> <input type="checkbox"/> The project demonstrates that exterior installation of public art is infeasible or undesirable ( <i>Urban Design, Policies 1.6.2, 1.6.3 and 1.6.4</i> )	-	-	-						
10) Utilities and associated support structures are installed underground or are appropriately screened from view by decorative architectural walls or landscaping ( <i>Electric Power and Other Fuels Sub-Element, Policy 2.1.7 and 2.1.8</i> )	-	-	-						
<b>PRESERVATION OF HISTORIC BUILDINGS AND SITES COMMITTEE (PHBSC) – Note: see also #9 above</b>									
11) The project meets the requirements of the University's Memorandum of Agreement with the State Division of Historical Resources because <input type="checkbox"/> The site is located adjacent to an Archaeological Site or within an Archaeological Sensitivity Zone ( <i>Urban Design, Policy 1.7.1</i> ); <u>AND/OR</u> <input checked="" type="checkbox"/> The project is new construction or a building addition located within the Historic District or Historic Impact Area depicted on Figure 1-2, Urban Design Element; <u>AND/OR</u> <input type="checkbox"/> The project includes renovation, rehabilitation or restoration of an existing structure that meets the definition of "historic property" described in Policy 1.5.4 of the Facilities Maintenance Element									
a) If "yes" for new construction or building additions, the project design is sensitive to the orientation and character defining features of existing structures in the Historic Impact Area ( <i>Urban Design, Policy 1.7.2</i> ); with a building height between 2 and 5 stories not to exceed the height of existing historically significant buildings in close proximity ( <i>Urban Design, Policy 1.3.7</i> )									



**Campus Master Plan Checklist**

EVALUATION CRITERIA	PROGRAMMING AND SITE SELECTION			COMBINE FOR DESIGN-BUILD					
				SCHEMATIC DESIGN			DESIGN DEVELOPMENT		
				YES	NO	NA	YES	NO	NA
<b>LAKES, VEGETATION AND LANDSCAPING COMMITTEE (LVLC) – Note: see also #8 above</b>									
12) <input type="checkbox"/> The project does not reduce the size of an area in the Conservation Future Land Use (Figure 2-1, Future Land Use); <u>OR</u> <input type="checkbox"/> The project mitigates the Conservation Future Land Use change per Conservation, Policy 1.4.11	X								
13) <input type="checkbox"/> The project (or any associated utilities or infrastructure) is not adjacent to or within a Conservation Future Land Use; <u>OR</u> <input checked="" type="checkbox"/> The project siting, orientation and landscaping minimize visual impact on the Conservation Area, preserve native vegetation and allow a graduated transition from developed areas to Conservation Areas ( <i>Conservation Element, 1.1.4</i> )									
14) The project minimizes impacts <u>and</u> conforms to the intent of the Conservation Area because the project is for new utilities or infrastructure (including exterior lighting and stormwater facilities) within a Conservation Future Land Use ( <i>Conservation, Policies 1.4.8, 1.4.9 and 1.4.10</i> ) – <i>Note: LVLC approval recommendation required</i>		X							
15) <input type="checkbox"/> The project is not within 50-feet of a wetland; <u>OR</u> <input checked="" type="checkbox"/> The project within 50-feet of a wetland minimizes impacts to wetlands and the required wetland buffers; <u>and</u> provides a minimum 35-foot setback and average 50-foot setback; <u>and</u> uses only native plants in a naturalistic landscape design within wetland buffers ( <i>Conservation, Policies 1.2.1, 1.2.2, 1.2.3, 1.2.4, and 1.2.5</i> )									
16) <input checked="" type="checkbox"/> The project is not within the 100-year floodplain; <u>OR</u> <input type="checkbox"/> The project within the 100-year floodplain addresses building elevation, compensating storage and off-site mitigation ( <i>Conservation, Policy 1.2.6</i> )	X								
17) <input checked="" type="checkbox"/> The project does not disturb any plants or animals identified as threatened and endangered species or species of special concern by federal and state agencies; <u>OR</u> <input type="checkbox"/> The project inventories such species and develops protection or relocation plans in coordination with appropriate local, state and federal agencies ( <i>Conservation, Policies 1.3.2 and 1.3.3</i> )	X								
18) <input type="checkbox"/> The project site does not impact an Open Space Connection identified in Figure 1-4, Urban Design Element ; <u>OR</u> <input checked="" type="checkbox"/> The project maintains, enhances or satisfactorily realigns the open space connection ( <i>Urban Design, Policies 1.2.4 and 1.3.2; and Transportation, Policy 2.2.5</i> )									
19) <input type="checkbox"/> The project site is not within or adjacent to an Open Space Enhancement Priority area identified in Figure 1-5, Urban Design Element; <u>OR</u> <input checked="" type="checkbox"/> The project provides appropriate landscaping, hardscaping, and bicycle/pedestrian open space enhancement for the related Open Space Enhancement Priority area ( <i>Urban Design, Policy 1.4.2</i> )									
20) The project integrates with existing topography and natural features ( <i>Urban Design, Policy 1.3.11</i> )	X								

**Campus Master Plan Checklist**

EVALUATION CRITERIA	PROGRAMMING AND SITE SELECTION			COMBINE FOR DESIGN-BUILD					
				SCHEMATIC DESIGN			DESIGN DEVELOPMENT		
	YES	NO	NA	YES	NO	NA	YES	NO	NA
21) The project identifies any potential adverse affects, accommodates any increase in volume of runoff over the pre-development volume for a 72-hour period from the 100-year storm event, and provides a courtesy review to the City of Gainesville because the project is within the Hogtown Creek drainage basin ( <i>General Infrastructure Stormwater Sub-Element, Policy 1.3.5</i> )		X							
22) The project use trees, plant materials, exterior furniture, paving materials and walls to reinforce spatial organization and create "outdoor rooms" in functional open space adjacent to buildings, within the Urban Park Future Land Use, and along roadways, pedestrian connections and shared-use paths depicted in Figure 1-4 ( <i>Urban Design, Policies 1.3.3 and 1.4.1</i> )	-	-	-						
23) Stormwater retention facilities associated with the project (if any) are designed to be natural and curvilinear in outline with variable side slopes, smooth transitions to existing grade and planted with native vegetation ( <i>General Infrastructure Stormwater Sub-Element, Policies 1.2.4 and 1.2.5</i> )	-	-	-						
24) The project incorporates Best Management Practices and Low Impact Development design to address stormwater quality and quantity including pollutants, erosion and sedimentation ( <i>General Infrastructure Stormwater Sub-Element Policies 1.3.2, 1.3.3, 1.3.4 and 1.4.1</i> )	-	-	-						
25) The project satisfies UF Design & Construction Standards for tree protection, removal, relocation and mitigation ( <i>Urban Design, Policies 1.4.9, 1.4.10 and 1.4.12</i> ) – Note: LVLC approval recommendation required	-	-	-						
26) The project satisfies UF Design & Construction Standards for landscaping in parking lots and around buildings, and installation is concurrent with the appropriate building construction phase ( <i>Urban Design, Policies 1.4.13, 1.4.14 and 1.4.15</i> ) – Note: LVLC approval recommendation required	-	-	-						
<b>PARKING AND TRANSPORTATION COMMITTEE (P&amp;TC)</b> – Note: see also #18 and #19 above									
27) The project provides a traffic engineering study with a courtesy review by UF's host local governments because the project includes a parking structure or surface with at least 300 parking spaces located in Alachua County ( <i>Transportation, Policy 1.2.2 and 1.2.3</i> )		X							
28) <input type="checkbox"/> The project does not result in any significant loss of existing parking; <u>OR</u> <input type="checkbox"/> The loss of significant existing parking is mitigated - Note: Parking loss mitigation to be negotiated in consultation with the P&TC ( <i>Transportation, Policy 2.6.5</i> )	X								
29) The project satisfies UF Design & Construction Standards for bicycle parking including quantity, location and lighting with covering as feasible ( <i>Transportation, Policy 2.2.6</i> )	-	-	-						
30) <input type="checkbox"/> The project provides hot water showers and lockers for use by bicycle commuters; <u>OR</u> <input type="checkbox"/> The project demonstrates that hot water showers and lockers are infeasible ( <i>Transportation, Policy 2.2.13</i> )	-	-	-						
31) The project provides adequate parking to meet the needs of disabled persons, service and delivery vehicles necessitated by the building construction project ( <i>Transportation, Policy 2.6.5</i> )	-	-	-						



UF-653  
ARCHITECTURE BUILDING  
RENOVATION / REMODELING AND  
DCP COLLABORATORY

Programming  
August 2021

## Existing Architecture Building

- Built in 1979
- There were significant budget cuts from the time of design to construction of the architecture building. The scope of work was drastically reduced, and essentially, the occupants moved into a shell of a building.
- Significant water intrusion issues are a result of this incomplete design.
- The building is unacceptable in terms of the Florida Building Code compliance, ADA compliance and Life Safety.





### New Building:

- May be an addition or connected with covered walkway
- Respecting conservation
- Most likely 3 stories

### Program:

- 54,800 GSF
- Research Labs
- Digital Modeling
- Fabrication Space
- Teaching Space
- Creative Commons

### Schedule:

- Program approval
- Design team selection begins in August
- Construction in 2023



### Potential Tree Impacts:

- There are 7 trees that will be impacted
- There are an additional 2 trees that might be impacted





View South from the East-most Sidewalk





View West from the East-most Sidewalk





View South from the West-most Sidewalk





View Northwest







**White Ash - 5**



**Sycamore - 7**



**Live Oak - 4**



**Sycamore - 2**

7 Trees To Be Removed (trees #6 and #7 are likely Heritage)



**Sycamore - 6**



**Mulberry  
- 3**



**Mulberry  
- 8**



## 2 Trees To Likely Remain

**Heritage  
Magnolia  
- 1**



**Heritage  
Live Oak  
- 9**





### Parking Impacts:

- There are 4 parking spots at the Music Building
- We may need to rework them, but am not planning on removing any

### Sidewalk Impacts:

- The angled sidewalk will be impacted





# MP06556 Lake Wauburg North Shore - Waterfront Equipment Building

## LAKES, VEGETATION AND LANDSCAPING COMMITTEE PRESENTATION – AUGUST 12, 2021





# MP06556 Lake Wauburg North Shore - Waterfront Equipment Building

## Context Map





## **LVL MOTION FROM JULY 8, 2021:**

**Approve the tree preservation and mitigation summary as presented and come back to the committee with ideas around stormwater mitigation that might include expanding the swale or other forms of material.**





# MP06556 Lake Wauburg North Shore - Waterfront Equipment Building

**Design team considered use of pervious hardscape materials but declined to recommend based on the following:**

- Existing water table due to proximity to Lake Wauburg. Geotechnical report estimates groundwater to perch up to  $\pm 2.5'$  below grade after periods of intense or seasonal rainfall.
- Existing hillside seepage issues.
- Upfront costs. Pervious concrete and pavers approximately double costs.
- Ongoing maintenance costs of cleaning & vacuuming pervious materials to ensure functionality.





# P06556 Lake Wauburg North Shore - Waterfront Equipment Building

## 100% Construction Documents – ORIGINAL DESIGN

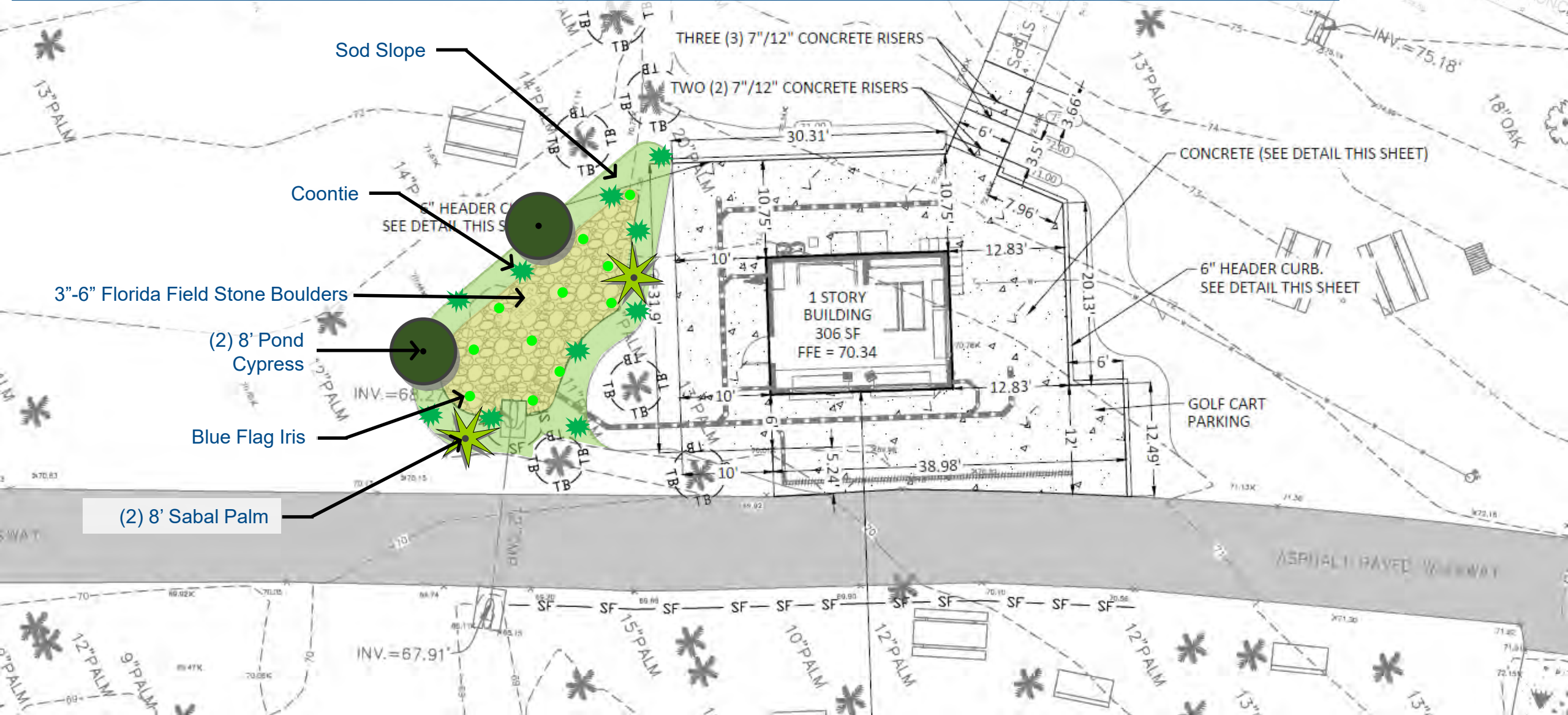
100% Construction Documents – ORIGINAL DESIGN



# MP06556 Lake Wauburg North Shore - Waterfront Equipment Building

## 100% Construction Documents + BioSwale Concept – NEW DESIGN

### Treatment Area = ADDITIONAL 500 Cu.Ft. of VOLUME + NATIVE PLANTINGS





# MP06556 Lake Wauburg North Shore - Waterfront Equipment Building

## LAKES, VEGETATION AND LANDSCAPING COMMITTEE PRESENTATION – AUGUST 12, 2021


### RECOMMENDATIONS:

1. Approve modifications to approved Tree Mitigation Plan
2. Approve proposed stormwater mitigation improvements





Landscape Master Plan Amendments (Section 7) - LVL 8/12

Issue	Proposed Amendment
<p>No Fencing standard that incorporates brick columns. (Section 1.9, page 159).</p> <p>No standard for landscape bed protection fencing/post and chain fencing.</p>	<p>Add standards to allow this type of fencing:</p>  <p>Precincts: 1, S.1, 2</p> <p><b>Column Base:</b> Manufacturer: Cherokee Brick (904) 262-5280 <a href="http://www.cherokeebrick.com">www.cherokeebrick.com</a> Brick: 3 5/8" x 2 1/4" x 7 5/8" Wall Height: Varies to match fencing height Material: Clay Color &amp; Pattern: Red flashed range; running bond</p> <p><b>Column Cap:</b> Manufacturer: Cherokee Brick (904) 262-5280 <a href="http://www.cherokeebrick.com">www.cherokeebrick.com</a> Material: Clay Cap: Cast concrete cap; limestone</p>





Precincts 1 and S.1  
Details TBD.

Remove Circle rack and replace with silver Peak Rack with wider tire slots to maintain an option for Precincts 3 and 4.



Bike Accessories  
(Section 2.7, pg. 167)

The Circle Bike Rack (Image below) is not SITES compliant.

Existing Peak racks do not fit the standard tire size for electric bikes.



**BIKE RACK**  
**Manufacturer:** Anova  
**Style:** Circle Bike Rack / CIRCLEBRS2  
**Material:** Stainless Steel  
**Base:** Surface mount  
**Precinct:** 3, 4



Hardscape Standards (Hardscape section 1.1, pg. 150): and Bike/Pedestrian Ways (Section 1.2, pg. 153)



**CONCRETE**

- Material:** All concrete sidewalks shall be a minimum of 6-inches thick, reinforced with fiber or wire mesh conforming under the current American Concrete Institute standards
- Finish:** Floated and troweled with medium broom finish
- Color & Pattern:** Uncolored
- Control Jts:** Saw-cut to squared relief. All plans shall indicate control joint locations
- Expansion Jts:** Shall be in accordance with current ANSI and ASTM standards
- Precinct:** All precincts



**COLORED CONCRETE**

- Material:** See CONCRETE (above) for material specifications
- Finish:** Floated and troweled; finish may vary upon approval
- Control Joints:** All plans shall indicate control joint locations.
- Expansion Jts:** Shall be in accordance with current ANSI and ASTM standards.
- Precinct:** 3, 4
- Note:** Avoid the use of colored concrete above underground utilities, given the difficulties inherent in color matching.



**SHARED-USE PATH - PERVIOUS ASPHALT**

- Specifications:** Shared-use paths should be designed in accordance with the latest edition of the "Florida Greenbook," issued by the Florida Department of Transportation and the "Guide for the Development of Bicycle Facilities" published by the American Association of State Highway Officials (AASHTO).
- Width:** 10' min. 12-20' preferred depending on volumes and mix of bike/ped
- Material:** Pervious Asphalt
- Base:** 1 1/4" minimum thickness asphalt on minimum 4" limerock or crushed concrete base.
- Color:** As shown
- Precinct:** 1, 2, 3, 4



**SHARED-USE PATH - CONCRETE; W/ DIVIDER**

- Specifications:** Shared-use paths should be designed in accordance with the latest edition of the "Florida Greenbook," issued by the Florida Department of Transportation and the "Guide for the Development of Bicycle Facilities" published by the American Association of State Highway Officials (AASHTO).
- Width:** 10' min. 12-20' preferred depending on volumes and mix of bike/ped
- Material:** Concrete
- Base:** Minimum 6-inches thick concrete slab with fiber or wire mesh reinforcement
- Color:** As shown
- Precinct:** 1, S.1, all other locations where the bike/pedestrian way is shared with vehicles such as a service drive.

Remove 'Material' section from Concrete and Colored Concrete and 'Base' section from Shared-Use paths. Refer to Division 32 of the UF Design and Construction Standards.

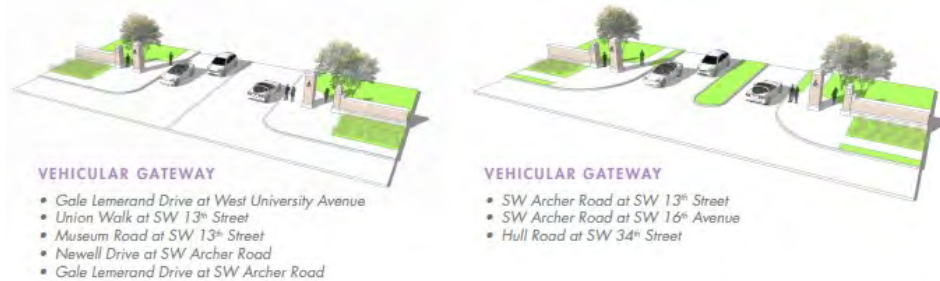


There is currently not a gateway standard for major vehicular campus entries on major roads (ex. Archer Rd).

Add a 'Major Vehicular Gateway' standard.

Precincts: 1,2,3 & 4

Consistent with existing vehicular gateway design except for 1) adding brushed aluminum lettering consistent with the UF Design & Construction Standards and 2) up-lighting on the sign.



Root Barrier (Section 1.11, pg. 160)

Remove model number & replace with product details. Add a note that says, 'or equivalent'.



Manufacturer: Citygreen  
(888) 999-3990  
[www.citygreen.com](http://www.citygreen.com)

- Thickness of 1000g/m2 or 1mm.
- Made from 100% recycled HDPE

Precinct: All

\*Or Equivalent

Bollards in Dumpster Areas  
(Section 2.8, pg., 168)

Write an exception for bollards located in back-of-house areas

\*Bollards located in out-of-site dumpster or service areas are not subject to the decorative bollards on this page.



**\*Discussion Item**

Memorials (Section 2.9, pg. 169)



**MEMORIAL**

Specifications: 24" x 24" x 27"  
Brick:  $3\frac{5}{8}" \times 7\frac{5}{8}" \times 2\frac{1}{4}"$   
Manufacturer: Cherokee Brick  
(904) 262-5280  
www.cherokeebrick.com  
Material: Brick, Concrete, Bronze  
Precinct: 1, S.1, 2

Maintenance concerns

Add a note regulating the location of memorials to be within landscape beds or tree rings.

**\*Discussion Item**

Pollinator Plants

Are there any plant species that should be added to or removed from the plant palettes?

Analysis of plants within the Landscape Master Plan Plant Palettes

<https://gardeningsolutions.ifas.ufl.edu/plants/trees-and-shrubs/trees/>

<https://gardeningsolutions.ifas.ufl.edu/plants/trees-and-shrubs/shrubs/>

<https://gardeningsolutions.ifas.ufl.edu/plants/trees-and-shrubs/palms-and-cycads/>

<https://gardeningsolutions.ifas.ufl.edu/design/gardening-with-wildlife/bee-plants.html>